

8. PEDESTRIAN AREAS & STREETSCAPE STANDARDS

PROJECT GENERAL STANDARDS

General project standards shall conform to the Town of Eagle Municipal Code except as designated herein.

GENERAL PEDESTRIAN AREA AND STREETSCAPE STANDARDS

The following section defines the pedestrian area and streetscape standards for the development, including general standards, paving, site amenities, and streetscape standards.

OVERALL PEDESTRIAN AREAS AND STREETSCAPE CONCEPT

Pedestrian Areas shall encourage and facilitate the ease of use of pedestrians through paved walks, storefronts, trails, plazas. Pedestrian areas shall provide accessibility options for the disabled. Pedestrian areas shall incorporate paving, decorative paving, and other amenities to help create a 'sense of place' for the development. (See figure 8.1.1) See page 66 for Pedestrian Trail and Connections Plan.



fig. 8.1.1

Coordinated site furnishings, such as benches, litter receptacles, and pedestrian lighting shall be located throughout to help unify the development. Additional amenities such as outdoor fireplaces, seatwalls, fountains and sculpture may also be used to add to the overall experience of the development. (See figure 8.1.2)

Streetscape elements shall include street trees, lighting, walks, and crosswalks. The streetscape shall be a unifying element for the development. (See figures 8.1.3 and 8.1.4)

The project shall use turn of the century style streetscape features such as "gas light" style lamps, street clocks, ornate benches, street sign posts and cobblestone pavers.

Ample bicycle parking racks shall be provided throughout the development. Bicycle racks shall be located out of the main path of pedestrian travel and shall not, when used, present an obstruction to pedestrian travel. Pedestrian hard surface sidewalks shall connect all bicycle rack locations and the main pedestrian paths of travel.

Areas for convenient, safe, secure, and sanitary 'dog parking' shall be incorporated into the development, where possible.

PEDESTRIAN AREAS

Pedestrian Paving

Internal concrete/paver sidewalks shall be a minimum of 5' wide.



fig. 8.1.2



fig. 8.1.3

All pedestrian paving areas shall be designed to be accessible in accordance with ADAAG requirements. (See figure 8.1.5)

Pedestrian sidewalk connections should be adjacent to all storefronts. Additionally, at least one (1) pedestrian sidewalk access should exist from the parking areas to the storefronts. Travel along the sides of buildings, or through interior building courtyards and vestibules to get to the storefronts is acceptable. Pedestrians may also travel through parking lots to pedestrian walks on parking lot pavements.

Specialty paving may be used throughout the pedestrian plazas, streetscape, and storefronts including, but not limited to, masonry pavers, colored concrete, textured/stamped/colored concrete and stone. Main colors will be earth tones with color accents for architectural compatibility.



fig. 8.1.5



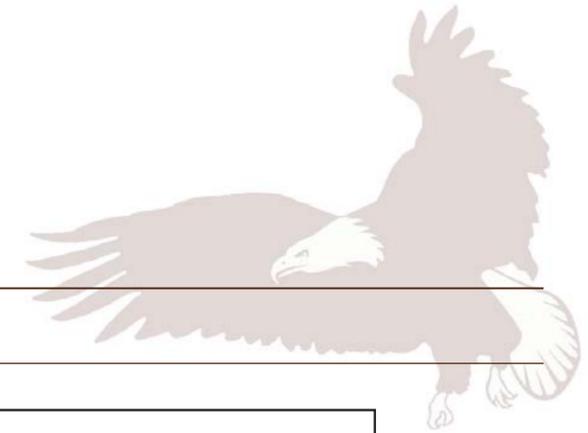
fig. 8.1.4



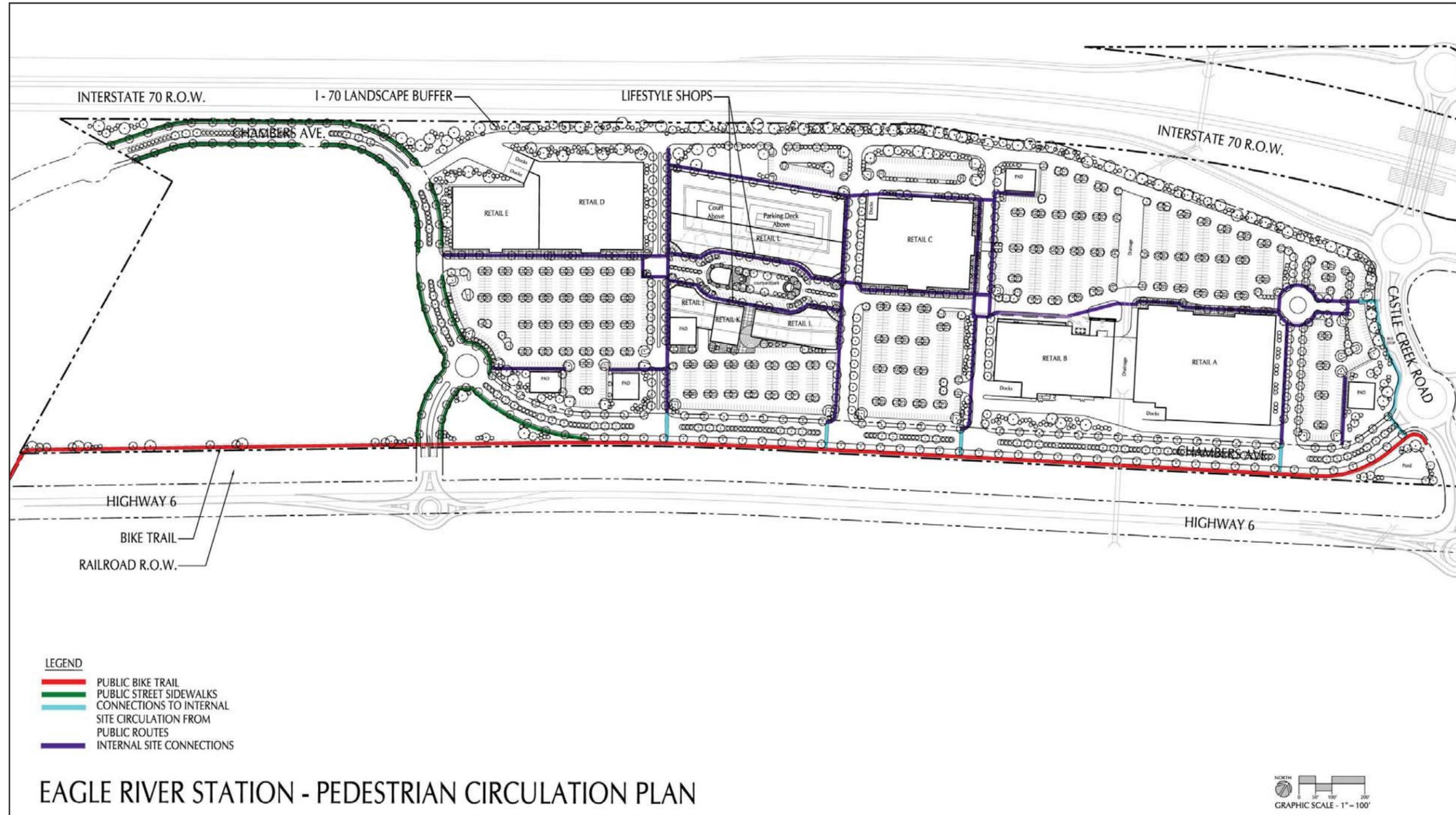
fig. 8.1.7



fig. 8.1.6



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED



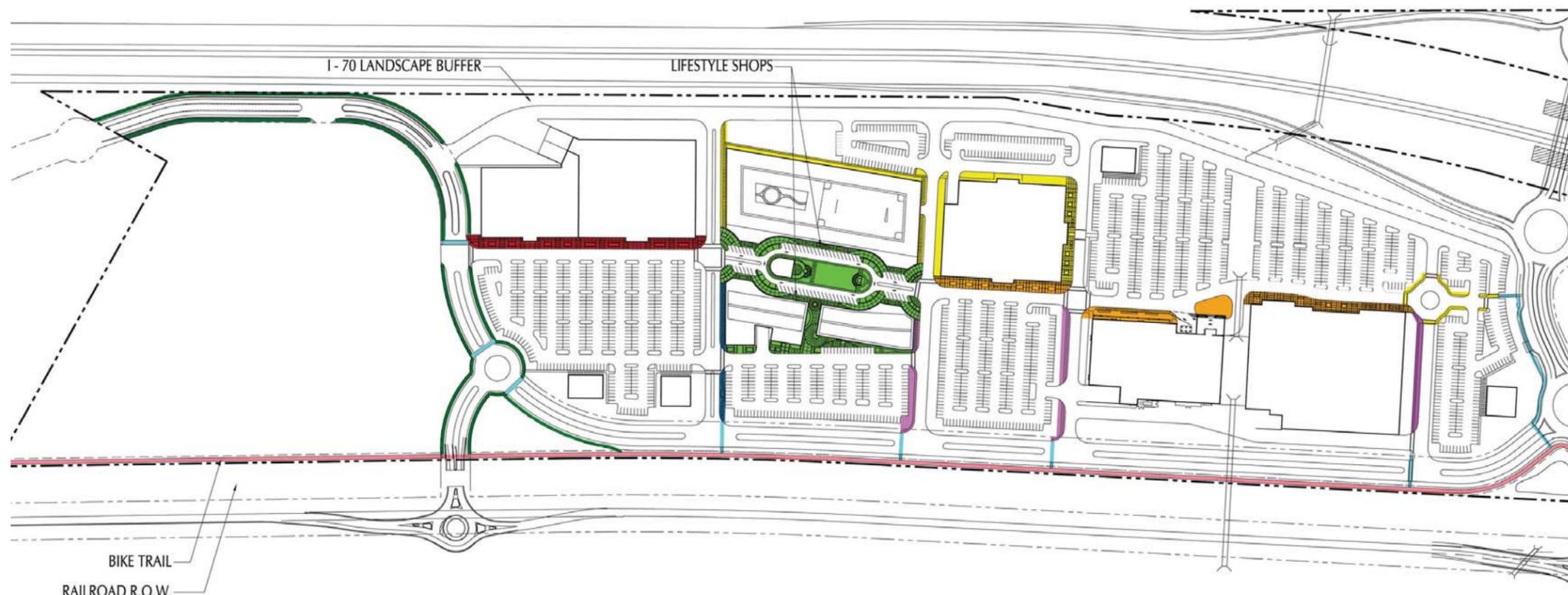


8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

PEDESTRIAN CONNECTIONS

Pedestrian Circulation Hierarchy Standards

The pedestrian circulation hierarchy standards set out to define what the most important pedestrian ways are, and provide basic standards for them. Primary connectors define the major pedestrian routes from the public walk and trail systems, to the heart of the project, the Mixed Use Lifestyle area. Additional connectors are also provided.

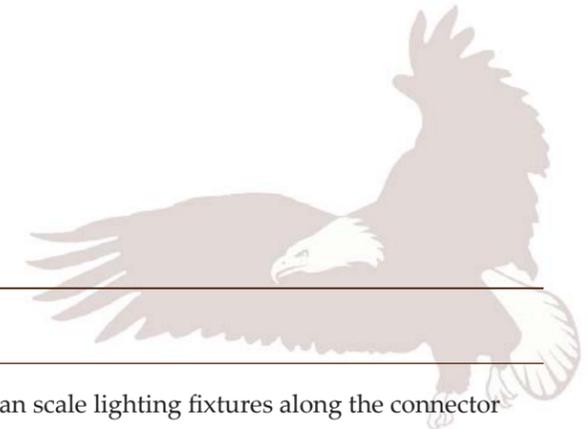


LEGEND

- PUBLIC BIKE TRAIL
- PUBLIC STREET SIDEWALKS
- CONNECTIONS TO INTERNAL SITE
- CIRCULATION FROM PUBLIC ROUTES
- PRIMARY - EAST/WEST CONNECTOR
- PRIMARY ZONE - HEART OF PROJECT
- SECONDARY - EAST/WEST CONNECTOR
- PRIMARY - NORTH/SOUTH CONNECTOR
- SECONDARY - NORTH SOUTH CONNECTOR
- TERTIARY SITE CONNECTIONS

EAGLE RIVER STATION - PEDESTRIAN CIRCULATION HIERARCHY





8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

Primary East/West Connector

The Primary east west connector provides the significant pedestrian connection between the Mixed Use Lifestyle heart of the project and the Mixed Use Planning Area west of Chambers Avenue which is planned as a second phase of the development. The design of this pedestrian corridor should facilitate the east west movement of pedestrians from and to locations in either of the above referenced Planning Areas. This pedestrian area also should be designed to serve the needs of customers of the stores located in this Commercial Retail Planning Area and the movement of people from the parking lots to the entrances of these stores buildings. Key design elements should include landscaping, pavement designs, sidewalk furniture and pedestrian scale lighting that encourages the above referenced functions. Other design considerations should include orientation elements both on and off site that provide visual support to these movements.

Standards

Width: The Primary East/West Connector shall be an average of 20 feet in width from back of curb to the nearest point on the building façade with a minimum width of 15 feet. Reductions to this minimum width may be considered at store entries in order to accommodate building projections, trellaces or other architectural features, provided the east/west orientation of this corridor is maintained.

Materials: The Primary East/West Connector shall contain the following material enhancements along its length:

Paving: Approximately 30% of the connector shall be enhanced paving, including any combination of pavers, stamped concrete, colored concrete, textured concrete, or natural stone. The remaining may be finished concrete.

Seating: Provide a minimum of (4) seating opportunities along the connector. They may consist of any combination of benches, seat walls, or other structures made for pedestrian seating.

Lighting Provide pedestrian scale lighting fixtures along the connector that ties in to the overall lighting system of the development and facilitates the movement of pedestrian in an east-west fashion as well as from the parking lots into the entrances of the stores.

Landscape: Landscape planters, beds, and/or pots shall be provided along the connector, as needed to direct pedestrian flow, provide for interest, and to soften the overall effect of the corridor. Street trees shall be provided along the connector, provided they do not conflict with the entries, signage and branding of the adjacent tenants.

Secondary East/West Connector

The Secondary East/West Connectors provide connections from the CR East to the MUL heart of the project. These connections shall provide for a comfortable area for pedestrians to move with ease from parking lots to building entrances and along storefronts, and should contain enhancements in paving, landscape, seating, and lighting.

Standards

Width: The Primary East/West Connector shall be an average of 15' in width from back of curb to nearest point on the building façade. It may be wider as required or desired.

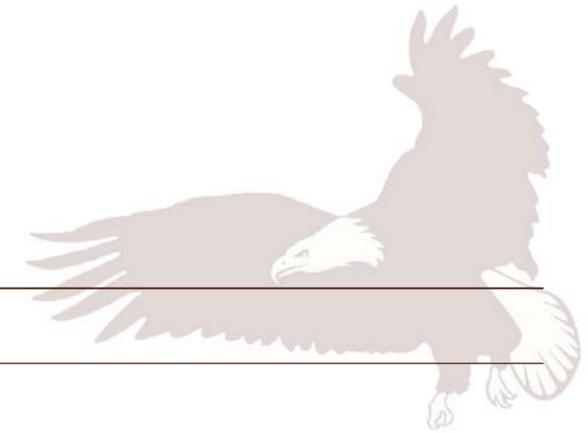
Materials: The Primary East/West Connector shall contain the following material enhancements along its length:

Paving: Approximately 20% of the connector shall be enhanced paving, including any combination of pavers, stamped concrete, colored concrete, textured concrete, or natural stone. The remaining may be finished concrete.

Seating: Provide a minimum of (2) seating opportunities along the connector. They may consist of any combination of benches, seat walls, or other structures made for pedestrian seating.

Lighting: Provide pedestrian scale lighting fixtures along the connector that ties in to the overall lighting system of the development. The utilization of banners and graphics is encouraged.

Landscape: Landscape planters, beds, and/or pots shall be provided along the connector, as needed to direct pedestrian flow, provide for interest, and to soften the overall effect of the corridor. Street trees shall be provided along the connector, provided they do not conflict with the entries, signage and branding of the adjacent tenants.



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED



MUL PLANNING AREA COMMUNITY GREEN SPACE

Primary Pedestrian Zone – Heart of the Project

The Primary Pedestrian Zone/Heart of the Project is defined as the pedestrian spaces in the MUL. These spaces shall provide a high-quality walking, strolling, shopping, and outdoor dining environment adjacent to and enhancing the buildings and overall experience. The zone should provide varied, interesting and comfortable area for pedestrians to move with ease from parking to building entrances and along storefronts, and should contain enhancements in paving, landscape, seating, and lighting. Additionally, plazas and amenities should be incorporated where possible to enhance the overall quality of the development.

Standards

Width: Primary Pedestrian Zone/Heart of the Project shall vary, but a minimum of 10' shall be provided with a minimum of 8' in front of all storefronts from back of curb to nearest point on the building façade. It may be wider as required or desired.

Materials: Primary Pedestrian Zone/Heart of the Project shall contain the following material enhancements along its length:

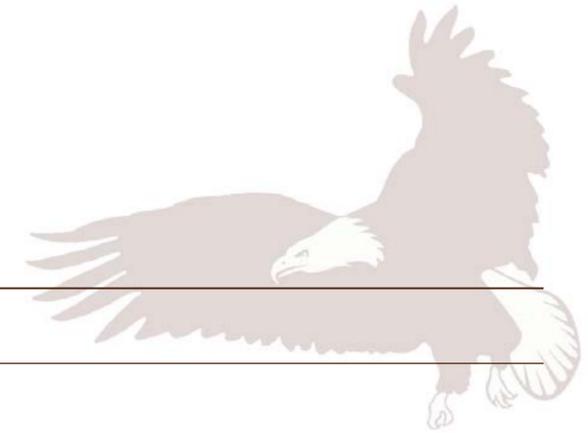
Paving: Approximately 30% of the connector shall be enhanced paving, including any combination of pavers, stamped concrete, colored concrete, textured concrete, or natural stone. The remaining may be finished concrete.

Seating: Provide a minimum of (10) seating opportunities in the zone. They may consist of any combination of benches, seat walls, or other structures made for pedestrian seating and shall be designed as an integral part of the streetscape.

Lighting: Provide pedestrian scale lighting fixtures along the connector that ties in to the overall lighting system of the development. The utilization of banners and graphics is encouraged.

Landscape: Landscape planters, beds, and/or pots shall be provided in the zone, as needed to direct pedestrian flow, provide for interest, and to soften the overall effect of the Zone. Street trees shall be provided along the connector, provided they do not conflict with the entries, signage and branding of the adjacent tenants.

Central Green: A central green shall be provided in the central median between the north and south buildings. This green shall contain street trees, landscape turf, landscape beds, sidewalks, and other amenities as appropriate. It shall be fully integrated with the building also located in the median.



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED



PRIMARY NORTH/SOUTH CONNECTOR

Primary North/South Connector

The Primary North/South Connector provides the significant connection between the MUL heart of the project, to the public hike/bike trail (Eco Trail) south of Chambers Avenue. This connection should provide for a wide, comfortable area for pedestrians to move with ease from the MUL storefronts to the bike trail. It should contain enhancements in paving, landscape, seating, and lighting. In addition, it should include provisions for integrated exercise and stretching stations.

Standards

Width: The Primary North/South Connector shall be a minimum of 20' in width, and wide enough to accommodate an 8' minimum width walk, connecting to crosswalks (as defined in the PUD) across all drives and roadways. In addition, an area for the installation of a stretching and exercise station, shall be provided adjacent to the main walk.

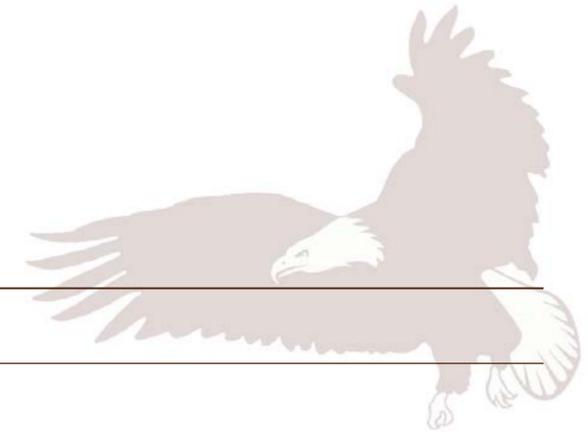
Materials: Primary North/South Connector shall contain the following material enhancements along its length:

Paving: Approximately 30% of the connector shall be enhanced paving, including any combination of pavers, stamped concrete, colored concrete, textured concrete, or natural stone. The remaining may be finished concrete.

Seating: Provide a minimum of (3) seating opportunities along the connector. They may consist of any combination of benches, seat walls, or other structures made for pedestrian seating.

Lighting: Provide pedestrian scale lighting fixtures along the connector that ties in to the overall lighting system of the development. The utilization of banners and graphics is encouraged.

Landscape: Landscape planters, beds, and/or pots shall be provided along the connector, as needed to direct pedestrian flow, provide for interest, and to soften the overall effect of the corridor. Street trees shall be provided along the connector, provided they do not conflict with the entries, signage and branding of the adjacent tenants.



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

Secondary North/South Connector

The Secondary North/South Connectors provides additional connections from the public bike/hike trail (Eco Trail) south of Chambers Avenue to the MUL and CR East. These connections should provide for a safe, easy to navigate, comfortable path for pedestrians to move with ease from the hike/bike trail (Eco Trail) to the MUL and CR East storefronts. It should contain enhancements in paving, landscape, seating, and lighting. In addition, it should include provisions for integrated exercise and stretching stations.

Standards

Width: The Secondary North/South Connector area shall be a minimum of 15' in width, and wide enough to accommodate an 5' minimum width walk, connecting to crosswalks(as defined in the PUD) across all drives and roadways. In addition, an area for the installation of one stretching and exercise station shall be provided adjacent to the main walk of each of the two westerly North /South connectors.

Materials: Secondary North/South Connector shall contain the following material enhancements along its length:

Paving: Approximately 15% of the connector shall be enhanced paving, including any combination of pavers, stamped concrete, colored concrete, textured concrete, or natural stone. The remaining may be finished concrete.

Seating: Provide a minimum of (2) seating opportunities along the connector. They may consist of any combination of benches, seat walls, or other structures made for pedestrian seating.

Lighting: Provide pedestrian scale lighting fixtures along the connector that ties in to the overall lighting system of the development. The utilization of banners and graphics is encouraged.

Landscape: Landscape planters, beds, and/or pots shall be provided along the connector, as needed to direct pedestrian flow, provide for interest, and to soften the overall effect of the corridor. Street trees shall be provided along the connector, provided they do not conflict with the entries, signage and branding of the adjacent tenants.

Tertiary Site Connections

The Tertiary Site Connections provide additional pedestrian connections throughout the site, adjacent to storefronts. These connections should provide for a safe, easy to navigate, comfortable path for pedestrians to move along the storefronts and to the adjacent parking lots. It should contain enhancements in paving, landscape, seating, and lighting.

Standards

Width: Tertiary Site Connections shall be a minimum of 10' in width, and wide enough to accommodate an 5' minimum width walk, connecting to crosswalks(as defined in the PUD) across all drives and roadways.

Materials: Tertiary North/South Connectors shall contain the following material enhancements along their length:

Paving: Approximately 10% of the connector shall be enhanced paving, including any combination of pavers, stamped concrete, colored concrete, textured concrete, or natural stone. The remaining may be finished concrete.

Seating: Provide a minimum of (2) seating opportunities along the connector. They may consist of any combination of benches, seat walls, or other structures made for pedestrian seating.

Lighting: Provide pedestrian scale lighting fixtures along the connector that ties in to the overall lighting system of the development. The utilization of banners and graphics is encouraged.

Landscape: Landscape planters, beds, and/or pots shall be provided along the connector, as needed to direct pedestrian flow, provide for interest, and to soften the overall effect of the corridor. Street trees shall be provided along the connector, provided they do not conflict with the entries, signage and branding of the adjacent tenants.



8. PEDESTRIAN AREAS & STREETSCAPE STANDARDS CONTINUED

Site Amenities

Site amenities include but are not limited to, bike racks, benches, tables, trash receptacles, ash urns, specialty lighting and freestanding planters. The site amenities shall compliment the architectural treatments of the development. All bike racks, benches, tables, trash receptacles, and ash urns shall be the same manufacturer make, model and color, throughout the development. (See figures 8.2.1 and 8.2.2)

Freestanding planters may be included for perennials and annuals that are changed seasonally. (See figures 8.2.3, 8.2.4 and 8.2.5)

The use of native boulders and rock material is encouraged.

Overhead trellises, tent structures, or other sun-screening architectural treatments are encouraged to provide pedestrian shade. (See figure 8.2.6)

SPECIFIC STANDARDS BY PUD PLANNING AREA

Additional specific standards for hardscape, streetscape and landscape will be unique for each Planning Area. Those standards are as follows:

CR – COMMERCIAL RETAIL PLANNING AREA

- a. Pedestrian paving areas shall be designed to provide clean, efficient and easy access to storefronts, circulation patterns and accessibility. (See figure 8.2.7)
- b. A minimum of 5'-0" walk shall be placed adjacent to all curbs at parking areas and pedestrian drop off/pick up areas.
- c. A minimum 8'-0" sidewalk shall be located directly adjacent to storefronts for circulation and accessibility.

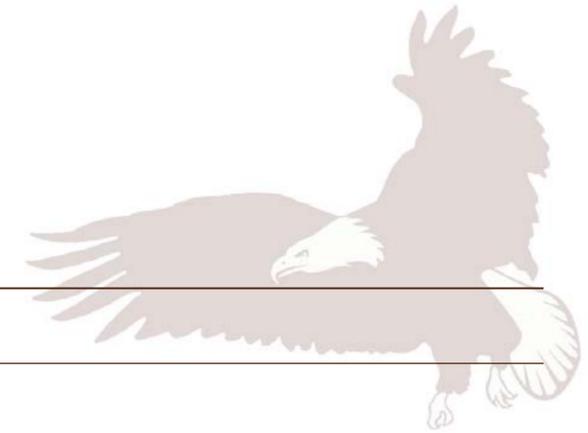
MUL – MIXED USE LIFESTYLE PLANNING AREA

- a. Pedestrian paving areas shall be designed to provide clean, efficient and easy access to storefronts, circulation patterns and accessibility.
- b. Paving areas where needed, shall be provided a minimum of 5'-0" along the back of curbs along parking head in parking stalls, and collection points along storefronts to provide safe drop off/pickup for pedestrians.
- c. A minimum 8'-0" sidewalk shall be located directly adjacent to all storefronts, 5'-0" along all side and rear facades. (See figure 8.2.9)

MU – MIXED USE PLANNING AREA

The MU Planning Area is a second phase of development, pedestrian and streetscape standards for this planning area will be prepared and submitted for review by the town as an addendum to this PUD Guide and PUD Development Plan at a subsequent stage in the development review process. Pedestrian and streetscape standards will be consistent with the General Pedestrian and Streetscape Standards and with the overall design approach and level of quality established in the design guidelines specific to the CR and MUL Planning Areas.





8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

STREETScape STANDARDS

In general, vehicular circulation within the development shall follow the Town of Eagle's design standards and the MUTCD, (latest version) and the AASHTO Policy on the Geometric Design of Streets and Highways (latest version).

The cross-sections as shown on pages 76-77, shall illustrate the typical cross sections for streets within the project.

Traffic calming measures shall be implemented where practicable to ensure a safe pedestrian environment, and based upon the approval of the Town Engineer.

All required on-street parking shall meet the Town of Eagle Public Works Department standards.

Pedestrian crosswalks shall be designated at vehicular intersections which have adjoining pedestrian walks. Cross walks shall be designated with a change in material scoring, color, or type. Allowable cross walk materials include colored and scored concrete, and colored and stamped concrete. The developer shall choose a material, layout and design that is harmonious with the rest of the development and that is safe for cars and vehicles. All pedestrian crosswalks shall be handicap accessible.

All pedestrian sidewalks and paths shall be un-obstructed for at least 5 feet in width, remain unobstructed to a height of at least 8 feet, and not be located in areas for snow placement, and generally follow a linear flow (no sudden unexpected directional changes). All canopies, signs, awnings must be at least 8 feet above the ground.

STREETS AND VEHICULAR ACCESS

Street improvements, site access, and traffic control shall be determined by the developer and Town of Eagle based on the traffic study. See page 75 for Roadway Classifications.

The developer shall submit improvement plans with roadway cross-sections that comply with the Town of Eagle's pavement standards to the Town Engineer for approval. The installed pavement sections shall comply with the approved improvement plans. The plans shall also include all necessary improvements to provide irrigation to landscape medians and islands within the right-of-way to the approval of the Town Engineer and Planning Director.

The design of the two Highway 6 Intersections shall comply with the requirements of the Colorado Department of Transportation and shall also be approved by the Town Engineer.

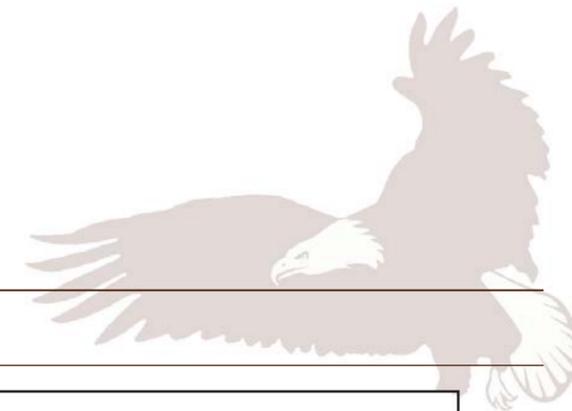
The developer shall complete improvements to Chambers Avenue from the current terminus at the west side of the proposed development to the western connection to Highway 6 and to the East Eagle Interchange Spur Road. These improvements will be subject to approval by Town Engineer and will include the following features:

- A) A multi-lane round-a-bout at US Highway 6.
- B) A railroad crossing that meets the design criteria of the Colorado Public Utilities Commission, the Colorado Department of Transportation, the Town of Eagle, and the Union Pacific Railroad, based on the typical section approved by the PUC. (See page 70 and 71)
- C) Left and right turn lanes at appropriate full public access intersections as determined by the traffic study.

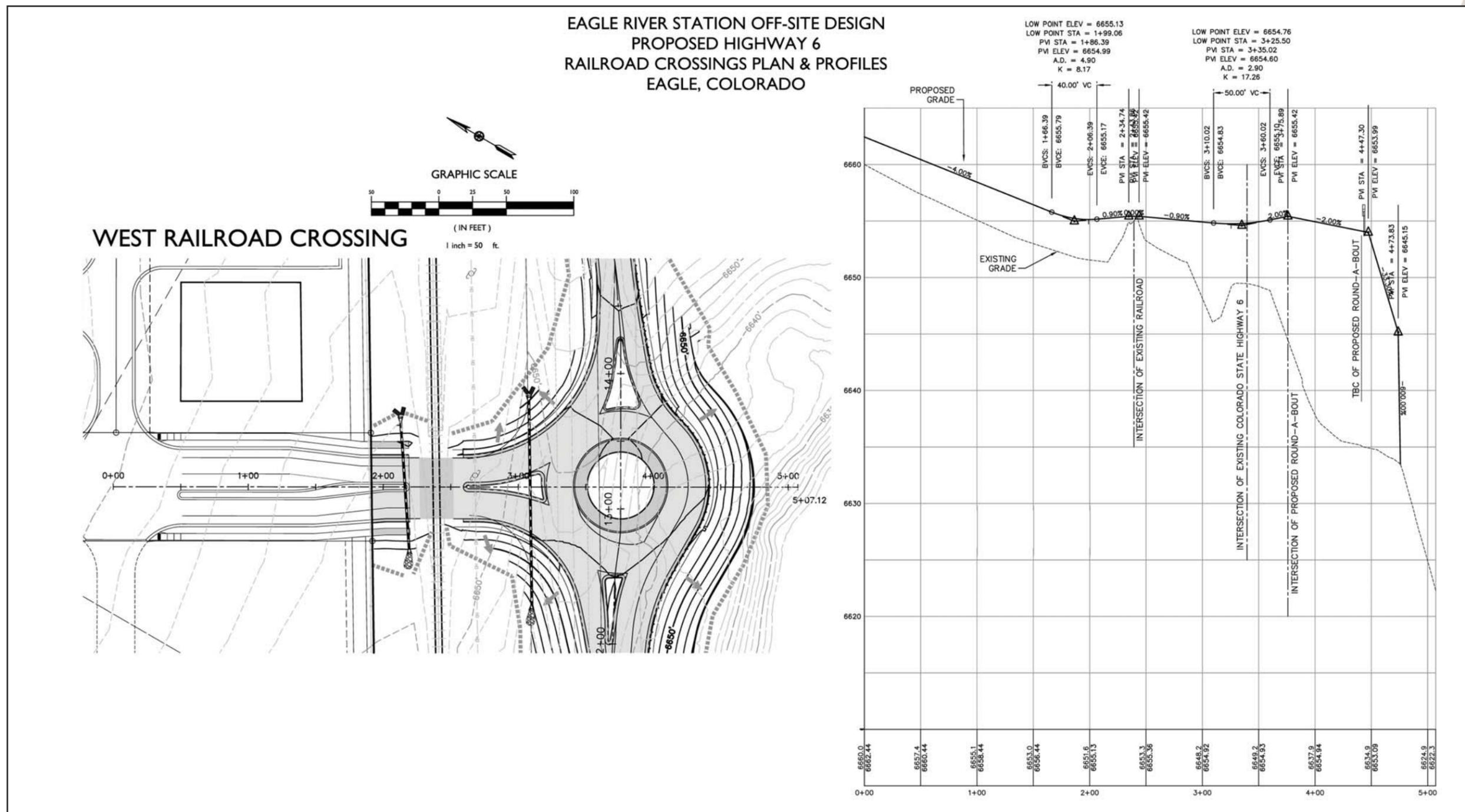
- D) A 5'-0" minimum sidewalk on both sides of the roadway as shown on the Pedestrian Circulation Plan on Page 66.
- E) A 6'-0" lane in both directions for automobile breakdowns and cyclists.
- F) A 16'-0" median, where appropriate for delineation of the travel lanes and for landscaping.
- G) A 12'-0" travel lane in both directions to accommodate automobiles.

The developer shall complete improvements for a new eastern interchange at Interstate Highway 70 and a connector road from the proposed interchange to US Highway 6, subject to the approval of the Town Engineer and Colorado Department of Transportation.

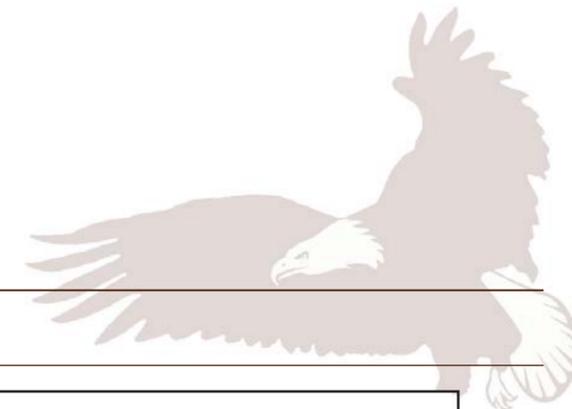
The interchange at interstate Highway 70 shall be completed prior to the issuance of any certificate of occupancy.



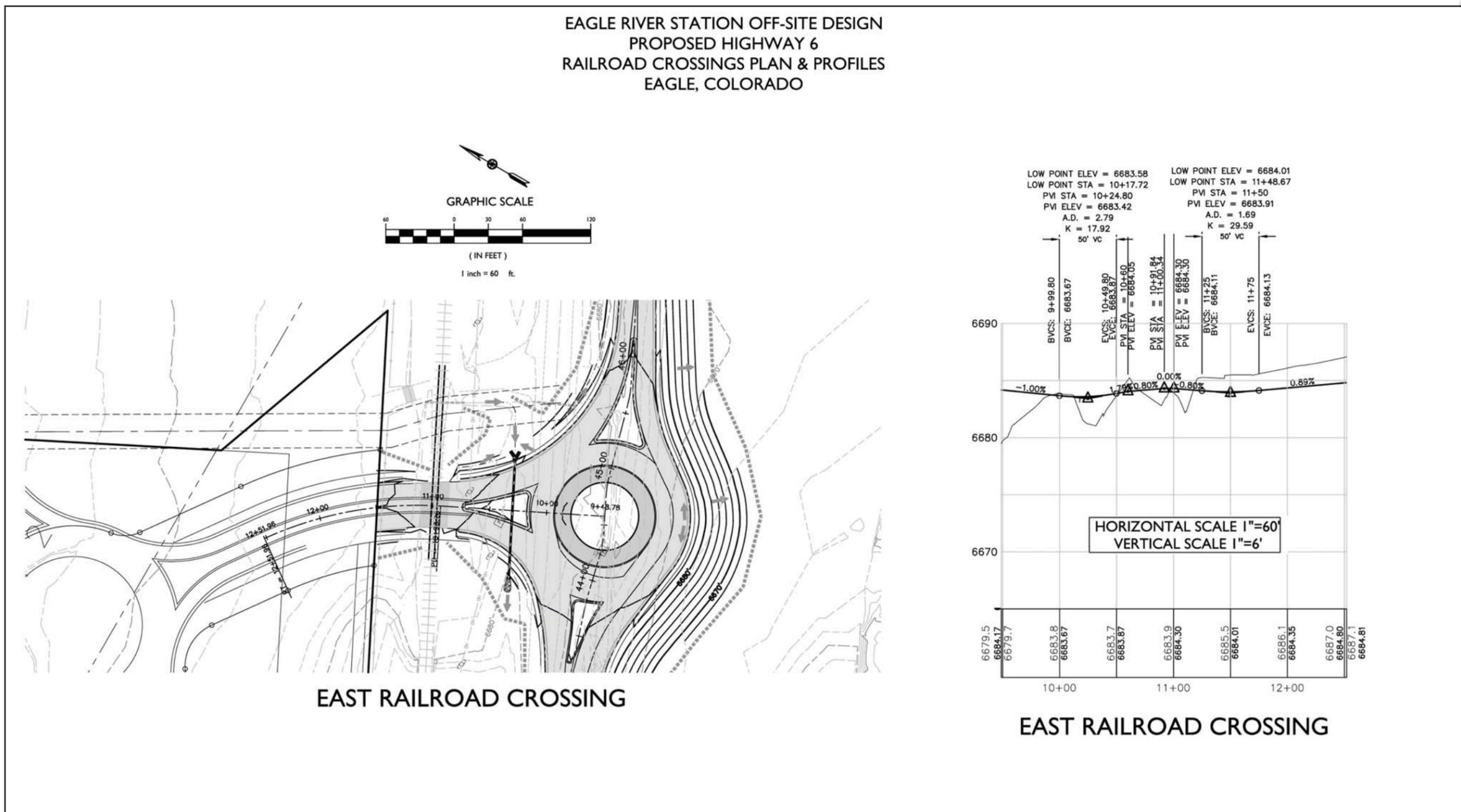
8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED



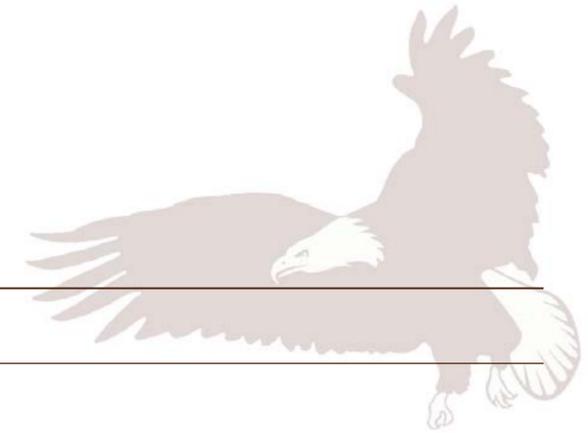
Railroad Crossing Diagram



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED



Railroad Crossing Diagram



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

MASS TRANSIT STOP AND FUTURE RAIL STOP

A location for the regional mass transit system (ECO Transit) bus stop is provided for the project. The Bus Stop shall provide for two buses and a mostly enclosed shelter. Bus Stop shall be completed and provided by the developer. Locations and designs for future stations and stops shall be approved by the Town of Eagle and Eco Transit. The developer shall coordinate with ECO Transit to determine a location that is acceptable to both. The stop shall fit into the overall theme of the project. A sidewalk connection shall be provided between the parking lot and the transit stop along the connector road. Bicycle racks shall be provided at the transit stop. The Developer agrees to set aside 30 spaces adjacent to the Eco Transit bus stop for shared parking use for the bus stop as park and ride stalls for transit users and patrons of Eagle River Station. The stalls shall be marked as such either with placard sign or painted on the pavement.

A location for a future rail stop shall be provided adjacent to the railroad in the MU area. The area shall be approximately 350' in length. Final location and design shall be determined in the future.

Locations and designs for future Eco Transit Stations and stops shall be approved by the Town of Eagle and ECO Transit.

PEDESTRIAN CONNECTIONS

A key element to the plan is the ability for pedestrians to move easily and safely through the development. The following standards define the pedestrian connections throughout the development.

The following pedestrian connections shall be provided throughout the site:

Trail – A public trail shall be provided through the property, located roughly north and adjacent to the Union Pacific Railroad right of way in the southern development buffer. A trail easement of 12' shall be provided to the Town, so the Town may have access to the trail for upkeep and maintenance. The trail shall be a 10' wide, asphalt trail, as outlined in Chapter 4 of the ECO Trails Plan. The trail may meander through the site, as necessary and as determined by the Developer to provide safe interaction with vehicular intersections and to provide better integration and interaction with the adjacent land uses.

The trail shall continue from the western boundary of the MU Planning Area of Eagle River Station, to the town's existing pedestrian and bicycle trail east of Nogal Road/US Highway 6. (Refer to the Eagle River Station Overall Master Site Plan for sidewalks and trails). The town shall be responsible for all acquisition of trail easements and rights-of way beyond the boundary of Eagle River Station. The Pedestrian System planned for the development will have a minimum of two (2) access connections to the ECO Trail.

The trail shall include a minimum of two "pull-out areas" that will provide a wider trail section to allow users to stop along the trail.



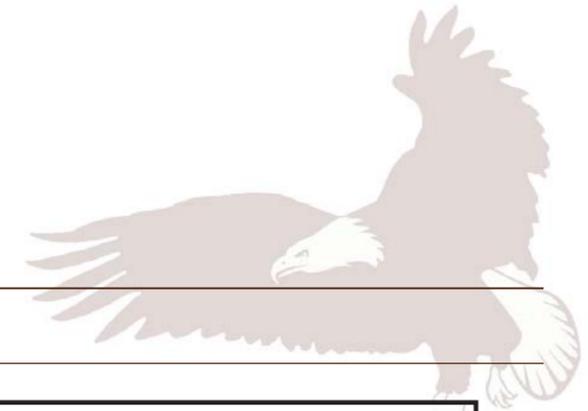
These areas will be located specifically to capture views to scenic areas or landmarks such as the Eagle River, Castle Peak or Red Canyon. Separated grade crossings (tunnels) shall be constructed where the recreation path ("ECO Trail") crosses the two connector roads to Highway 6. The type of construction for the tunnels and materials used will be determined during subsequent design phases but the tunnel shall be utilitarian, durable and safe.

Retail Store Fronts – The Developer shall provide pedestrian access along the retail store frontages and entries on a hard surface material (concrete, pavers, stone, etc). Minimum width from storefront face shall be five feet (5') and shall meet current ADAAG standards.

Parking Lot Connections – The Developer shall provide pedestrian connections from the parking lots to the buildings in the development. These connections shall be based upon the final site plan. The Developer shall mark vehicular and dedicated pedestrian intersection crossings with a change in material color and surface texture to alert vehicle operators of the possible presence of pedestrians in the roadway. Parking lot aisles shall not be so designated. (See figure 8.4.1)

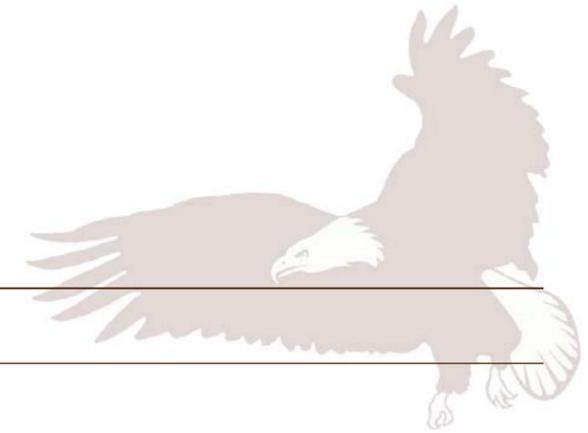
Chambers Avenue – The Developer shall provide a 5' sidewalk on each side of Chambers Avenue from the western property line to the ECO regional trail. Where Chambers Avenue is adjacent to I-70 and the required landscape buffer, the sidewalk shall be counted as a part of the landscape buffer requirement. The developer shall provide sidewalk connections to the Chambers Avenue Right of Way, from the public sidewalks within Eagle River Station.

Connections to Surrounding Public Rights of Way – A connection shall be provided to the new public road to the east of the development, and to the Highway 6 right-of-way.



8. PEDESTRIAN AREAS & STREETSCAPE STANDARDS CONTINUED





8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

PARKING LOT DESIGN

All circulation between different parking areas shall be within the property. Dead-end aisles will be avoided where possible.

A clear hierarchy of circulation will be evident. Major entry drives shall provide access to major parking fields. Pedestrian circulation through the parking areas shall be marked by landscaping, separate walking surfaces, and pedestrian scale lighting, directional signage, etc.

Parking areas and drives shall be separated from buildings by sidewalks and landscape strips of widths no less than required by the Eagle River Station Design Standards.

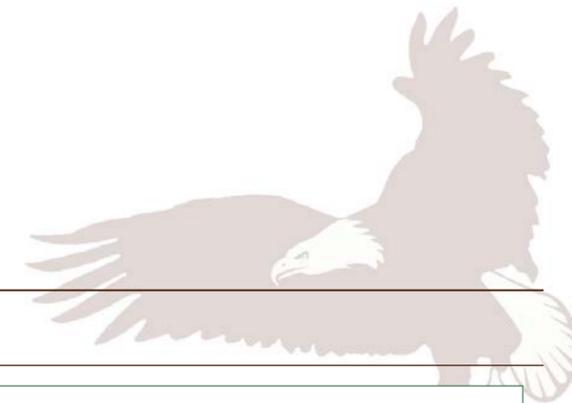
Shopping cart corrals shall be provided adjacent to retailers using shopping carts. The developer and/or tenants must demonstrate an effective shopping cart retrieval program to the satisfaction of the Town Planner prior to issuance of certificate of occupancy. Cart corrals shall be designed and constructed as necessary by each tenant requiring such areas. These permanent cart storage areas are to be constructed of masonry and decorative metal materials as used in the center. The design shall be submitted with the building design drawings for review and approval by the landlord and must take the overall development into consideration for the design cues. Masonry piers at the corners must be a minimum of 42" high for visibility and bollards at the corners for protection. For signage at cart corrals refer to Chapter 7. Cart corrals must be of a durable material and submitted for review as part of the entire design submittal.

Refer to Section 5 for landscape requirements for parking lots.

PARKING STANDARDS

All parking for the development shall conform to the Town of Eagle's Land Use Codes, except as follows:

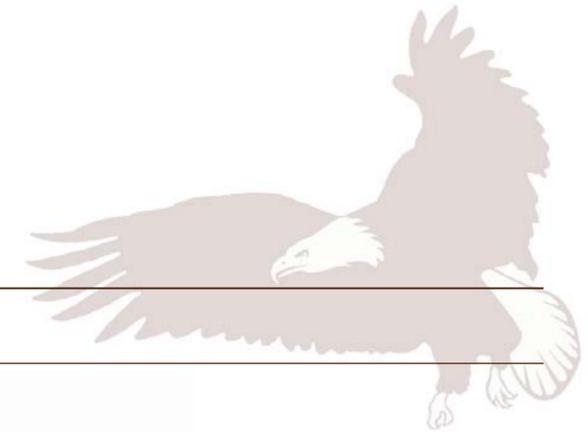
- Dimensions of all parking and aisle widths shall meet Town Standards.
- Any required residential parking located within a parking structure shall be restricted to residential use only and be secured by gates and or separate entries. Any required residential parking located outside of the structure shall be signed "Residential Parking Only".



8. PEDESTRIAN AREAS & STREETSCAPE STANDARDS CONTINUED

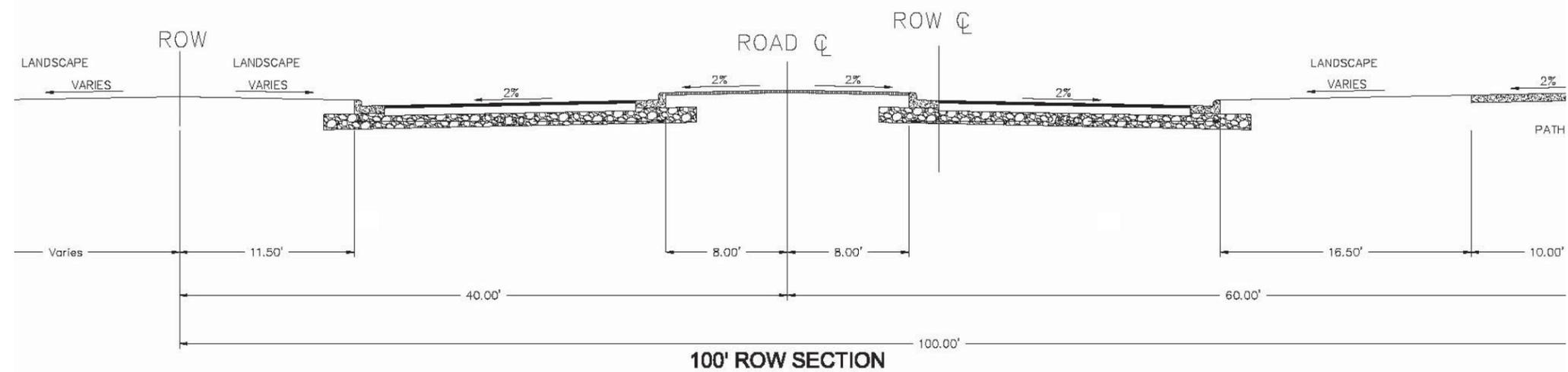
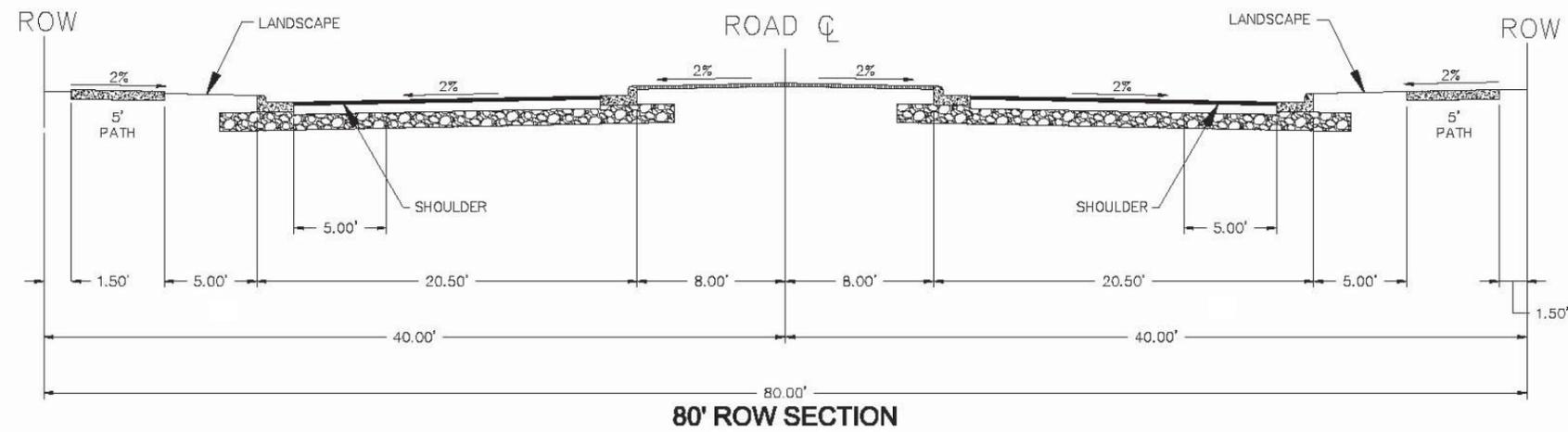


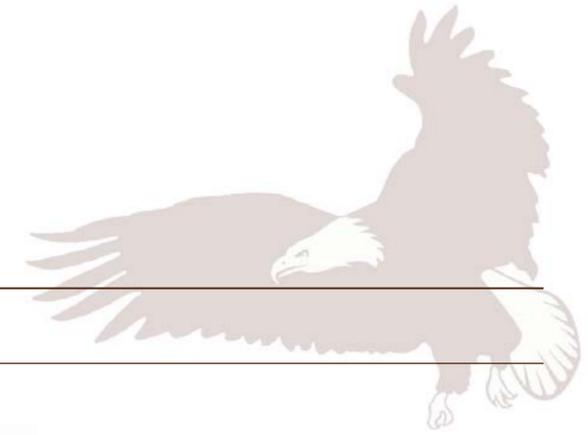
ROADWAY CLASSIFICATION DIAGRAM
SEE PAGES 64A AND 65 FOR ROADWAY SECTIONS.



8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

CHAMBERS AVENUE NOT TO SCALE





8. PEDESTRIAN AREAS & STREETScape STANDARDS CONTINUED

